



General guidelines **Load securing and related matters VPK Paper**

Version 1.0



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01. Foreword

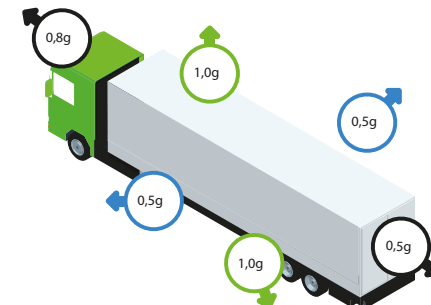
Securing goods plays an important role in daily shipping processes and ensures that the transport of goods by road can take place in safe conditions. This handbook aims to accurately explain to everyone involved in the loading process how to secure a load and also provides an overview of the additional conditions that paper-carrying trucks must meet.

This handbook has been developed in accordance with the following standards:

- **EN 12195-1: 2010 standard:** Load restraining on road vehicles - Safety - Part 1: Calculation of securing forces
- **EN 12641-2 standard:** Swap bodies - Tarpaulins - Part 1: Minimum requirements
- **EN 12642:** 2007, code L standard: Securing of cargo on road vehicles - Body structure of commercial vehicles - minimum requirements
- **EN 12195-2 standard, part 2:** Load restraint assemblies on road vehicles
- **EN 12640 standard:** Securing cargo on road vehicles

The instructions described below only apply to securing loads in closed curtainsiders and for the form-fitting loading of containers. This procedure ensures that the load can withstand the following forces:

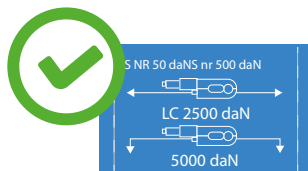
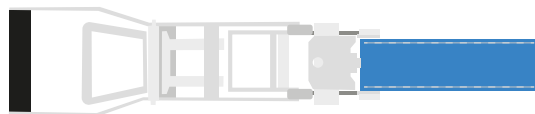
- Forward emergency stop – 0.8 g in the forward direction
- Backward stop – 0.5 g in the backward direction
- Lateral forces – 0.5 g in both directions
- S-Curve – 0.5 g in both directions
- Upward force 1.0 g
- Downward force 1.0 g



02. Required equipment

The following elements are required for load securing:

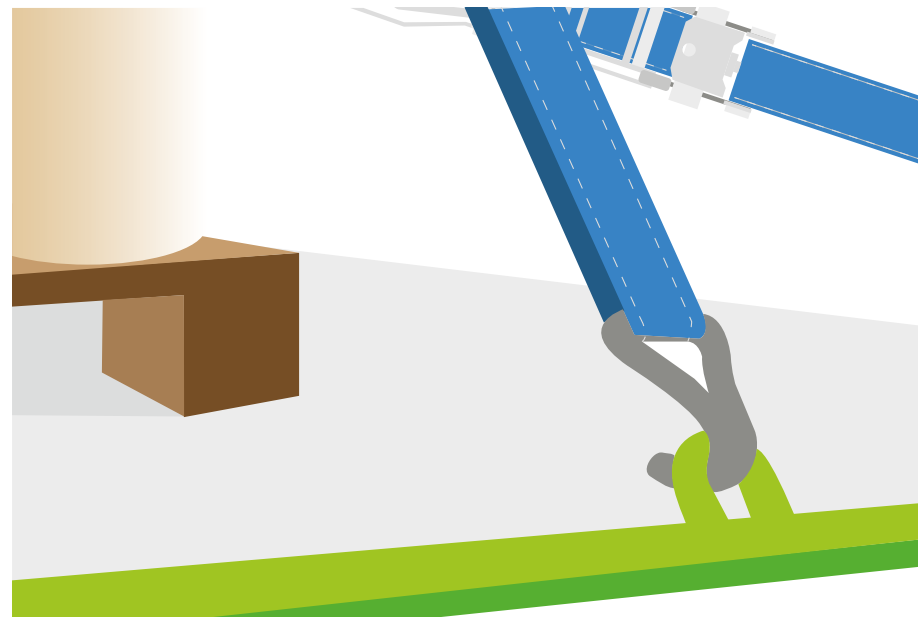
- 12 undamaged two-part ratchet straps with a minimum standard traction force (STF) of 500 daN and a minimum lashing capacity (LC) of 5000 daN.



- 24 corner protectors with a rounded corner profile, made of plastic and designed to fit the ratchet straps (see example below).



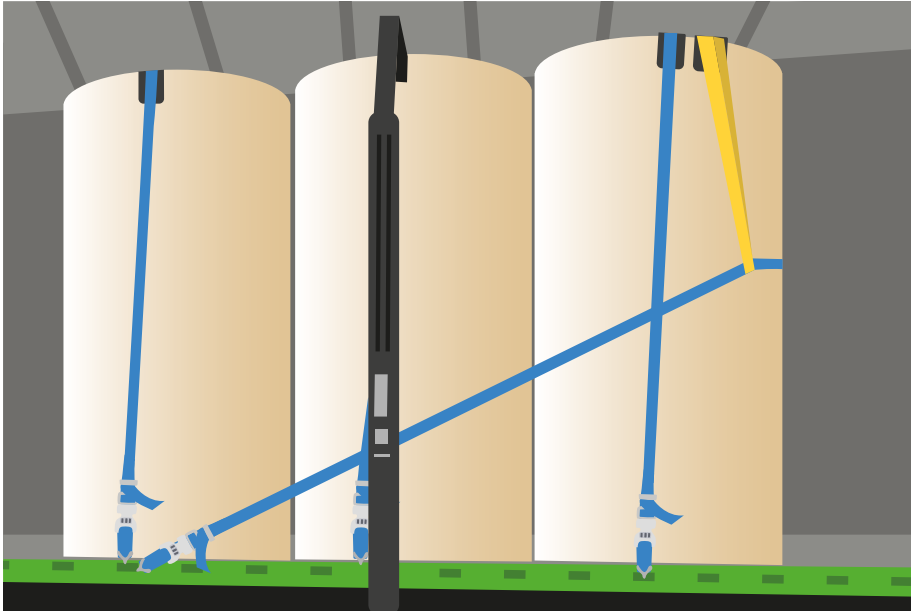
- Truck equipped with lashing points in accordance with EN 12195-2.



- Sufficient anti-slip mats for securing the load: (L x W x thickness)
 - Anti-slip mats with minimum dimensions of 300 x 300 x 8 millimetres and a minimum coefficient of friction (μ) of 0.6.
 - Anti-slip mats measuring 1400 x 150 x 8 millimetres with a minimum coefficient of friction (μ) of 0.6.
 - Or sufficient rolls of anti-slip mats measuring 5000 x 150 x 8 millimetres, with a minimum coefficient of friction (μ) of 0.6.



- 1 lifting strap for head lashing – to be used for loads that are not form-fitted against the headboard of the trailer. Dimensions: 4 metres in circumference and a minimum SWL (Safe Working Load) of 2000 kg.



Lashing strap with knots or crosswise placement of two lashing straps over the first roll is not permitted as an alternative.



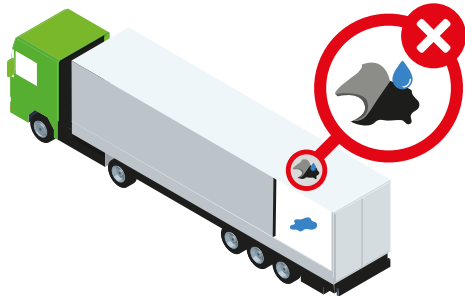
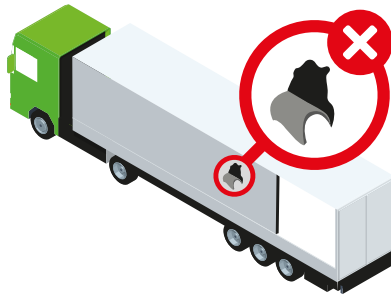
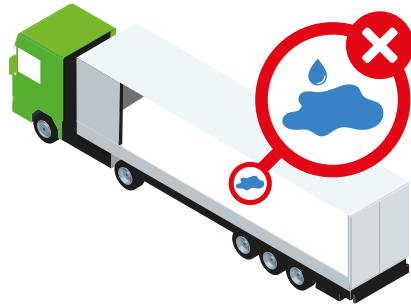
03. Responsibilities

Transport company

- The transport company must take the necessary measures to keep the entrusted goods in perfect condition during transport.
- The transport company must also provide all necessary means to meet the load-securing requirements (see 'Required equipment').
- Each trailer is equipped with a headboard that must be able to withstand at least 5000 daN, in accordance with EN 12642: 2007, code L.
- The trailer floor must be strong enough to support a forklift (5.5 tonnes) and the load (maximum 3.5 tonnes).
- Watertight tarpaulin and bottom.
- Removable or movable stanchions.
- Internal trailer height = roll height + 20 cm.

Truck driver

- Lashing the goods and installing the anti-slip mats and cleaning the loading floor are the responsibility of the truck driver.
- Flat, clean, dry and undamaged loading floor, without noticeable foreign odours.
- Floor free of any type of contamination such as dust, splinters, shavings, etc.
- Trailer must be GMP compliant (see 'Food safety').
- Depending on the number of rolls to be loaded, the driver must determine the distance between the headboard and the first roll based on the correct axle load distribution.
- Follows the safety rules on site (see point 5).



- Odours
 - The trailer must be completely odour-free.
- Mechanical parts
 - Doors, locking mechanisms, uprights, and locks must all be in good condition and ensure that the loading area can be fully sealed.

Any trailer detected as non-compliant will be rejected by VPK Paper



04. Special provisions on food safety (GMP)

- The paper will be used by our customers to manufacture packaging that can come into contact with food. We must ensure that no contamination (biological, chemical, physical) occurs during transport.
- While transporting the rolls of paper, it is strictly prohibited to have other cargo in the trailer. Any trailer where other goods are present will be refused.
- The booked trailers must be checked for possible contamination (e.g. floor contamination by a chemical product).



05. Safety rules

Wearing a high-visibility vest is mandatory.



Wearing steel-toed safety shoes is mandatory



There is a company-wide speed limit of 10 km/h



Smoking is prohibited within the company



Truck drivers and passengers are not permitted to leave the truck cab during loading and/or unloading activities



Unloading bales of waste paper using a walking floor is only permitted if the trailer is equipped with a remote control.



Making a fire is not allowed



Alcoholic drinks are not allowed



Loading and/or unloading operations may not begin until the driver is inside the cab (except in the case of a walking floor).



Use of a platform ladder, stick and/or ladder is mandatory.



06. Step-by-step procedure upon arrival

Inbound (loading and unloading waste paper)

Mandatory check-in

When you arrive at the unloading location, always report to reception. Always follow local instructions and only park in designated parking spaces.

Opening the trailer after being assigned a location

Once present at the unloading location the driver must open both sides of their trailer.

Waiting in the cab while loading and unloading

Unloading goods can be particularly dangerous. Always stay away from forklifts and working equipment, do not leave the driver's cab without permission, and make eye contact with others so as not to expose yourself and others to risks.

Truck cleaning

After unloading the goods, the driver must sweep out the truck before leaving the unloading area.

Additional requirements

Truck drivers are expected to understand one of the following languages:

- Dutch
- French
- English
- German

If this is not the case, with the result that instructions are not correctly understood and/or applied, VPK Paper will refuse the driver access to VPK.

Outbound (loading and unloading of goods)

Mandatory check-in

Upon arrival at the loading location, the driver must always log in at the reception. Always follow local instructions and only park in designated parking spaces.

Opening the trailer after being assigned a location + inspection of the trailer

The trailer must be inspected by the loader before loading the goods. The inspection is to ensure that the cargo is received undamaged and to meet the GMP requirements.

Waiting in the cab while loading and unloading

Loading goods can be particularly dangerous. Always stay away from forklifts and working equipment, do not leave the driver's cab without permission, and make eye contact with others so as not to expose yourself and others to risks.

Inspecting the goods

The truck driver must always visually check the condition of the goods after loading. If there is any damage, digital photos must be taken while the load is still in the vehicle. A remark must also be noted on the transport documents. Any discrepancies in the number of units received, compared to the consignment note, must be reported to VPK Paper.

Securing the goods

The load must be lashed down with straps to protect against sliding and tipping. The securing must be done so that the materials used to secure the load do not damage the load.

(See 'Loading methods')

Signing off the papers

The truck driver must correctly complete and sign all documents provided, such as:

- CMR consignment notes
- Registration sheets
- Waybills

Additional requirements

Truck drivers are expected to understand one of the following languages:

- Dutch
- French
- English
- German

If this is not the case, with the result that instructions are not correctly understood and/or applied, VPK Paper will refuse the driver access to VPK.

07. Loading methods

Vertical loading of paper rolls, form-fitting, in line

Method

Paper rolls are placed vertically from the headboard without gaps.

- a. Two anti-slip mats per roll (visible) are placed longitudinally on the outside under the roll (see point 8)
- b. Two corner protectors per roll
- c. Each roll must always be secured with a ratchet strap.
- d. The pre-tensioning force must be at least 500 daN
- e. Load all rolls in a form-fitting manner (see drawing below)
- f. The last roll is secured with a third anti-slip mat at the back



Vertical loading of paper rolls, NOT form-fitting, in line

Method

Paper rolls are placed vertically with a gap between the headboard and the first roll (= NON-form-fitting load, see drawing below).

- a. Two anti-slip mats per roll (visible) are placed longitudinally on the outside under the roll (see point 8)
- b. Two corner protectors per roll
- c. Each roll must always be secured with a ratchet strap.
- d. The pre-tensioning force must be at least 500 daN
- e. The front roll must be secured with a head lashing using an additional lifting strap.
- f. The last roll is secured with a third anti-slip mat at the back

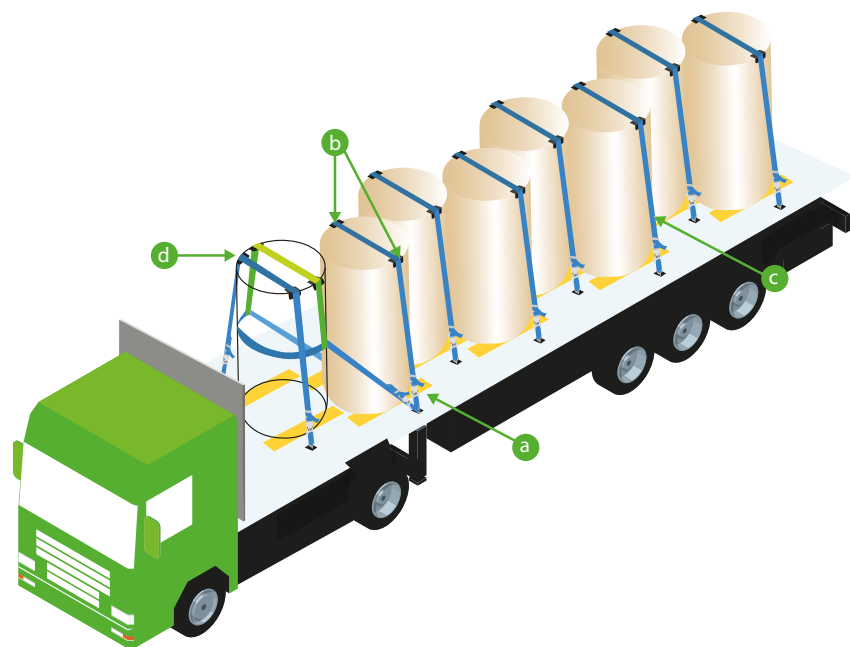


Vertical loading of paper rolls, NOT form-fitting, staggered

Method

Paper rolls are placed vertically with a gap between the headboard and the first roll.

- a. Two anti-slip mats per roll (visible) are placed longitudinally on the outside under the roll (see point 8)
- b. Two corner protectors per roll
- c. Each roll must always be secured with one ratchet strap
- d. The pre-tensioning force must be at least 500 daN
- e. Load all rolls in a form-fitting manner (see drawing below)
- f. The last roll is secured with a third anti-slip mat at the back
- g. The front roll must be secured with a head lashing using an additional lifting strap.



Vertical loading of paper rolls, form-fitting, staggered

Method

Paper rolls are placed vertically and staggered from the headboard without gaps.

- a. Two anti-slip mats per roll (visible) are placed longitudinally on the outside under the roll (see point 8)
- b. Two corner protectors per roll
- c. Each roll must always be secured with one ratchet strap
- d. The pre-tensioning force must be at least 500 daN
- e. Load all rolls in a form-fitting manner (see drawing below)
- f. The last roll is secured with a third anti-slip mat at the back



Vertical loading of paper rolls, form-fitting, in line and staggered

Method

Paper rolls are placed vertically and staggered from the headboard without gaps.

- a. Two anti-slip mats per roll (visible) are placed longitudinally on the outside under the roll (see point 8)
- b. Two corner protectors per roll
- c. Each roll must always be secured with one ratchet strap
- d. The pre-tensioning force must be at least 500 daN
- e. Load all rolls in a form-fitting manner (see drawing below)
- f. The last roll is secured with a third anti-slip mat at the back



Loading of pallets side by side or staggered

Method

Pallets are stacked 2 high and placed side by side or staggered.

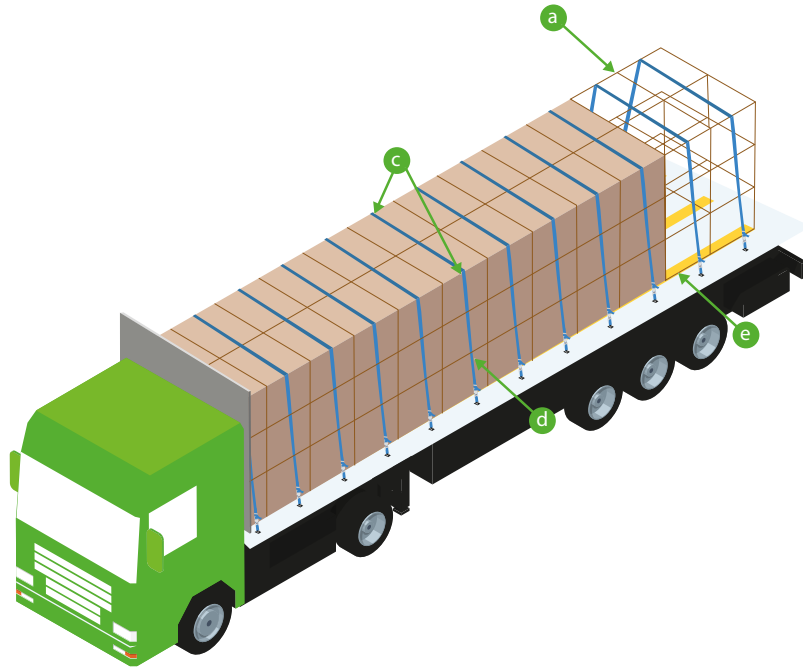
- a. Pallets must ALWAYS be anchored with a ratchet strap per pallet stack
- b. Two corner protectors per pallet stack across the truck width
- c. The pre-tensioning force must be at least 500 daN
- d. Form-fitting loading (see drawing below)



Loading bales of waste paper

Method

- a. Always load in a form-fitting manner (see drawing below)
- b. Only load complete stacks
- c. No corner protection required
- d. Ratchet straps required for all bales of waste paper (*)
- e. Two anti-slip mats per stacked bales of paper



(*) Note: The above loading method does not apply to 'walking floors' and trucks with a TUV certificate.

VPK Paper will provide the waste registration form that you, as a carrier, need to transport third-party waste.

Loading of containers

Inspecting the container

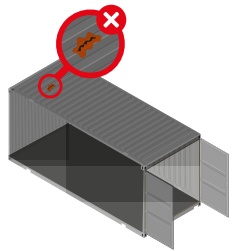
Container quality requirements are based on UCIRC (Unified Container Inspection and Repair Criteria) inspection criteria set by the International Chamber of Shipping. In addition to these instructions, industry-specific requirements must also be considered when accepting the container.

The inspection is to ensure that the recipient receives the cargo undamaged, but also to ensure AEO requirements for supply chain safety and security are met. The GMP regulations also apply to the loading of containers.

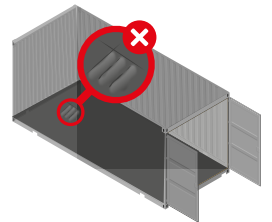
Uninspected containers may not be loaded

The following items must be checked by the forklift driver before loading the container:

No holes in the roof



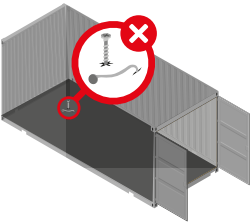
No damage or imperfections in the floor



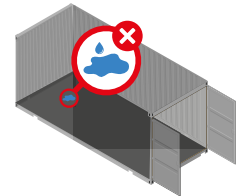
No foreign materials on the floor



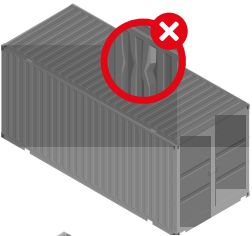
No protruding nails or screws in the floor



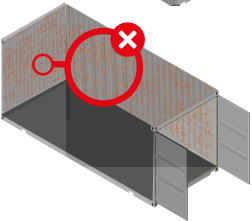
No liquids on the floor



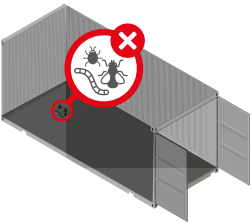
No wall damage present



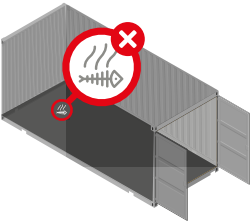
No rust or mould inside the container



No insects inside the container



No foreign odours present inside the container



Loading of form-fitting containers

When the container is loaded in a form-fitting manner, the container structure itself is used as load securing. The structure absorbs the dynamic forces that occur during the trip. Meaning: No additional load-securing elements have to be used for form-fitting cargo.

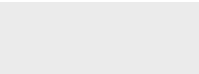
VPK Paper uses 40 ft containers.


Dimensions:

- Length: 12.02 m
- Width: 2.35 m
- Height: 2.39 m
- Usable capacity = 67.7 m³

The following configurations can be loaded in a form-fitting manner:

Height (mm)	1050	1300	1500	1650	1800	1850	2000
Ø 1450	11	11	11	11	11	10	10
Ø 1400	11	11	11	11	11	10	10
Ø 1350	12	12	12	12	12	12	12
Ø 1250	14	14	14	14	14	14	13

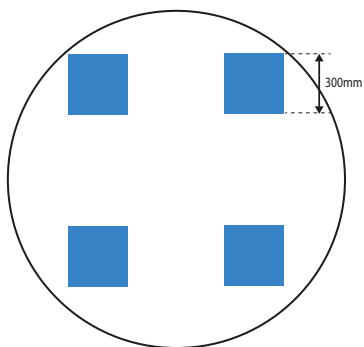
 form-fitting, where all rolls are loaded in a staggered manner

 form-fitting, where the first and last rolls are placed in the middle of the loading floor

08. Method for installing anti-slip mats

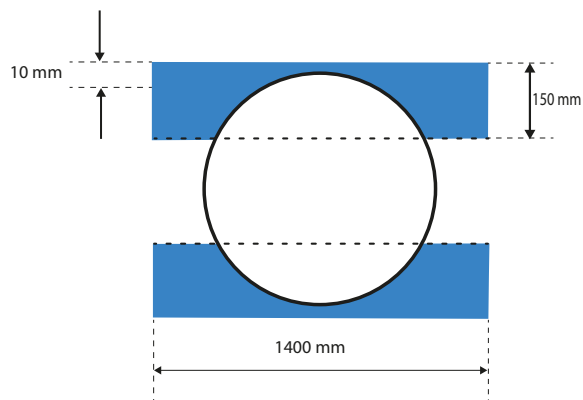
Placement of anti-slip mats measuring 300 × 300 × 8 mm (minimum 4 pieces per roll).

These anti-slip mats can be purchased from VPK Paper

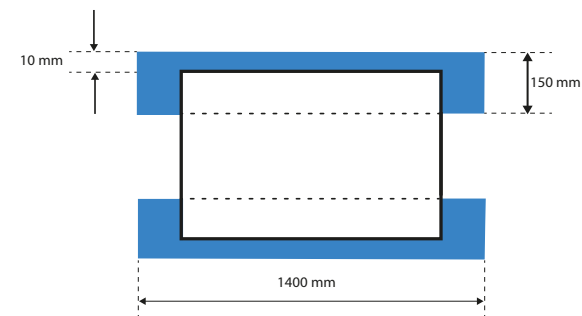


Placement of one-piece anti-slip mats measuring 1400 × 150 × 8 mm

These anti-slip mats can be purchased from VPK Paper
For rolls of paper (top view drawing):



For bales of paper (top view drawing):



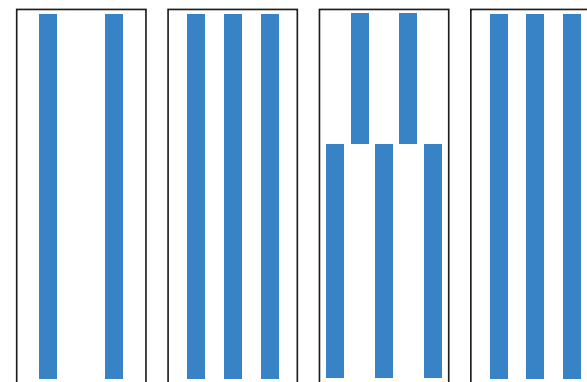
Only one-piece anti-slip mats are permitted

Placement of anti-slip mat rolls

It is possible to purchase anti-slip mats from VPK Paper for a fee that will be charged via 'self billing'.

Specifications of the anti-slip mats:

- Length × width × thickness → 1400 × 150 × 8 mm
- Coefficient of friction 0.6 μ
- Mat consists of 1 piece



10. Administration

a. Reservation of time slots

In order to guarantee a smooth flow of trucks and to minimise waiting times, drivers must reserve a time slot. Trucks that arrive outside their time slot will be assigned a new time slot if possible.

b. Safety instructions

PPE requirements: drivers must be in possession of the required personal protective equipment (PPE). VPK Paper does not provide PPE and drivers without the required PPE will not be able to access the site. (see 'Safety rules').

c. Parking restrictions

Trucks are not allowed to park in the nearby car parks of the factory (residential area) before or after loading. In exceptional cases, drivers may be permitted to park inside the factory, subject to approval and the provision of a parking permit.

d. Incident reporting

In the event of a collision or damage to equipment, an accident report form must be filled out by the driver and VPK Paper. This form will NOT be signed.

e. Load inspection

After loading, the driver must check whether the rolls are in good condition. If this is not the case, the driver must report this to the forklift driver. In the event of complaints, the CMR Convention applies.

f. Failure to load goods

If a truck cannot be loaded, the 'Failure to load goods report' form must be completed. Both parties (VPK Paper and truck driver) must receive a signed form. A complaint will not be dealt with without the inclusion of this report.

11. Opening hours

Oudegem opening hours

Address: Oude Baan 120 B-9200 Dendermonde:

- TP6 (P67 and Demag)
 - Monday 6am – Friday 6pm
 - Saturday by appointment
 - Closed on Sunday
- TP9 (PM1)
 - Monday 6am – Friday 4pm (between 10pm and 6am by appointment only)
 - Saturday by appointment
 - Closed on Sunday
- TP5 (Containers)
 - Monday 6am – Friday 6pm
 - Saturday by appointment
 - Closed on Sunday
- TP2 (waste paper)
 - Monday – Thursday 6am – 10pm
 - Friday 6am – 4pm
 - Night unloading Monday – Thursday: 4am – 6am
 - Saturday by appointment
 - Closed on Sunday

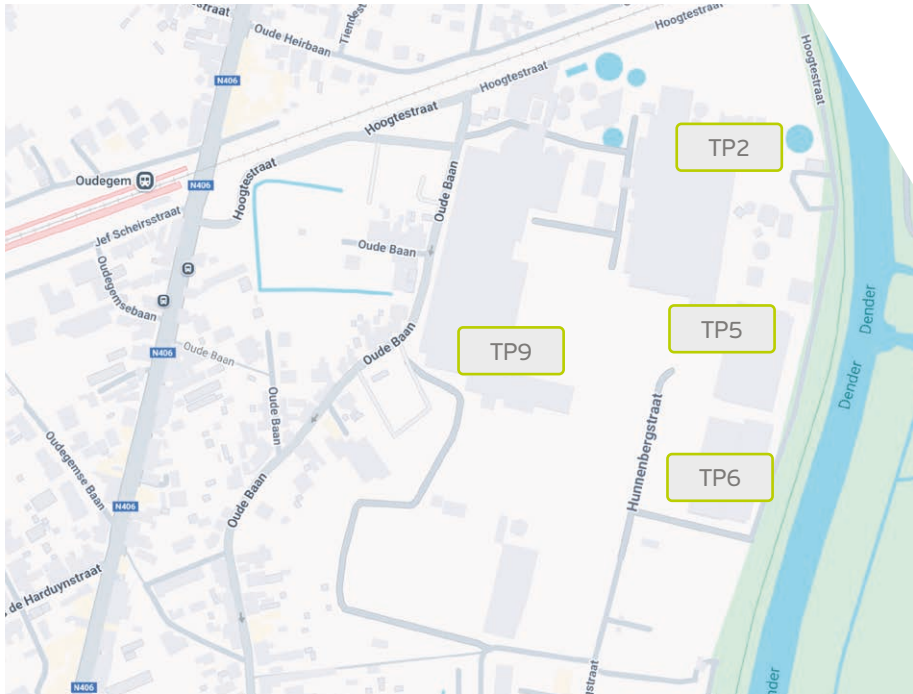
Dendermonde (Bosveld) opening hours:

Address: Bosveld 12 9200 Dendermonde:

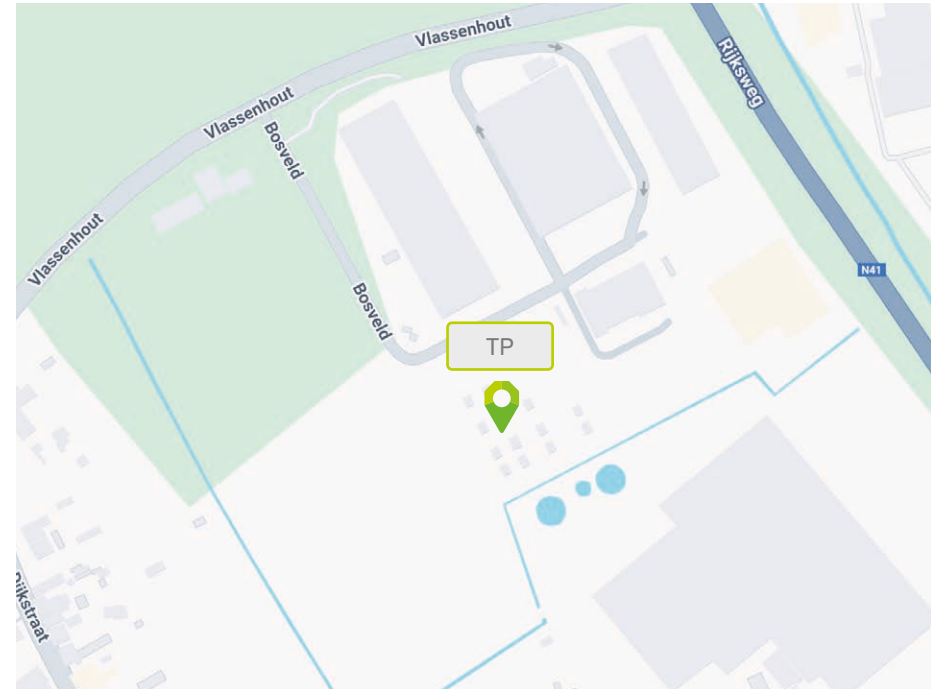
- By appointment

12. Entrance gates

Oudegem



Dendermonde



13. Glossary of terms

STF = standard traction force - preload force

LC = lashing capacity - tensile strength

daN = decaNewton - unit of force

SWL = safe working load - expressed in kilograms

FTL = Full truck load

PPE = Personal protective equipment

14. Certification

All the loading methods described in this manual have been tested according to the standards EN12342-B and EN12195 with ESTL as a certifying company under the following certificate numbers:

- 20140417_VPK_HV_14_01 → pallets with paper reels/discs on them
- 20140417_VPK_HV_14_02 → 250 in-line paper rolls
- 20140417_VPK_HV_14_03 → 250 staggered paper rolls
- 20140417_VPK_HV_14_04 → 250 paper rolls with head lashing (combined method)
- 20140417_VPK_HV_14_05 → 280 in-line paper rolls
- 20140417_VPK_HV_14_06 → 280 staggered paper rolls
- 20140417_VPK_HV_14_07 → 280 paper roll with in-line head lashing
- 20140417_VPK_HV_14_08 → 280 paper roll with head lashing (combined method)

Form stability with EUMOS as certifying company:

- Certificates:
 - Eumos 40509VPK001 4 layers
 - Eumos 40509VPK002 5 layers
 - Eumos 40509VPK003 6 layers
 - Eumos 40509VPK004 7 layers
 - Eumos 40509VPK005 8 layers
 - Eumos 40509VPK006 9 layers
 - Eumos 40509VPK007 10 layers
 - Eumos 40509VPK008 11 layers
 - Eumos 40509VPK009 12 layers
 - Eumos 40509VPK0010 14 layers

15. Contact details

- Inbound logistics → **0032479712283**
- Outbound logistics → **0032473852974**
- Transport planning → **sales.paper@vpkgroup.com**

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